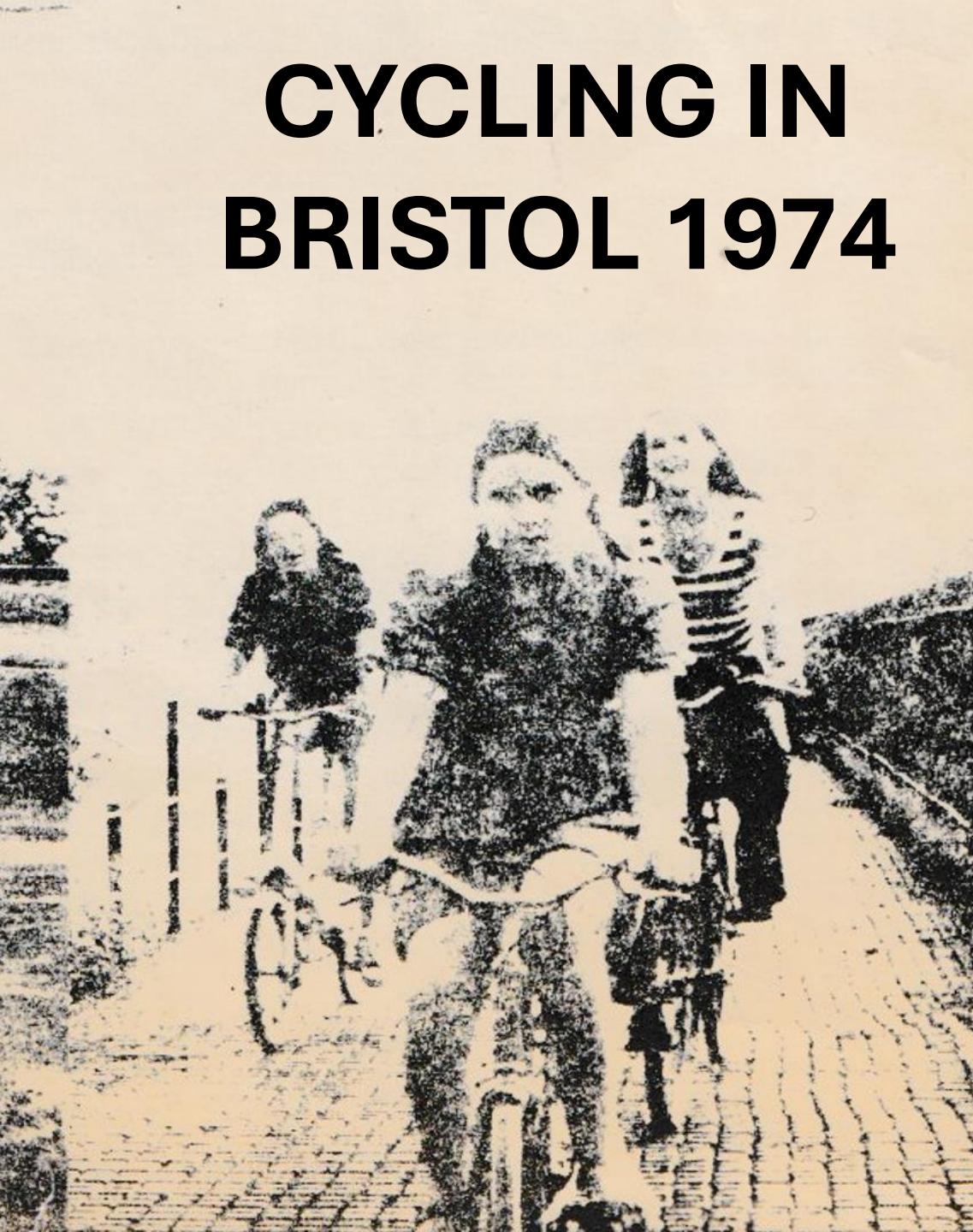


Cyclebag Sustainable Transport Ltd Sustrans Greenways and Cycleroutes



CYCLING IN BRISTOL 1974











THE FLOOD PLAIN

From Hanham Mills the valley widens out. The route follows the towing path, except where existing footpaths cut across loops of the river. There are occasions when this part will be flooded under two or three feet of water, but this is rare. The path will be built up a few inches (detail 11) above the waterlogged winter meadows and constructed on a firm nylon mat, to take the load off the occasional farmer's tractors. One problem that may arise will be that of livestock standing on the path just because it is dry, and also when the sun is out, warm. Again, there are plenty of precedents - on the meadows in Cambridge the animals and the public seem to co-exist without any record of conflict.

The illustration shows the towpath bridge over Siston Brook. From here another cyclepath could go northwards to link in Longwell Green, Oldland and Kingswood. The new Keynsham Road Bridge (detail 12) has wide spans to let the floods and cyclists through onto the old road that leads to Avonside House (illus.) This was built at the foot of the old mining tramway that brought coal down to barges from Coalpit Heath. The quickest way to the railway is the public footway, which is at present another gumboot job. The cycle path will run alongside field boundaries (detail 13) with cattle grids to allow farm access as needed.

Near the Bitton Tumulus the top of the railway embankment, which contains over 400,000 cubic yards of material, is low enough to be reached without difficulty.

We think that this is the highest point of the whole route from Bristol to Bath (detail 14).

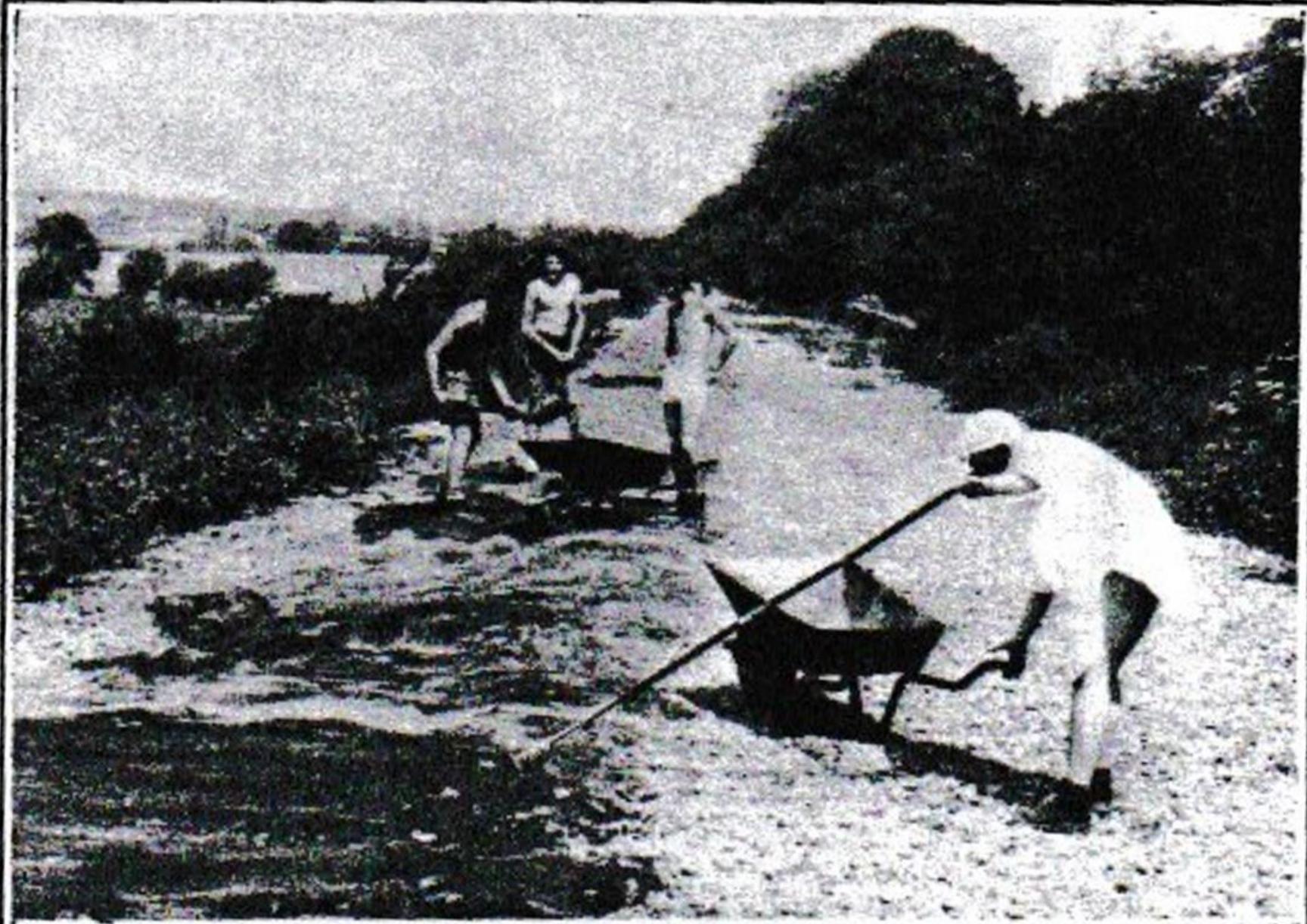
A STUDY OF DISUSED RAILWAYS IN AVON AND NORTH SOMERSET

**A REPORT FOR THE COUNTRYSIDE
COMMISSION PREPARED BY
JOHN GRIMSHAW & ASSOCIATES
AND RESEARCHED BY
CYCLEBAG BRISTOL**



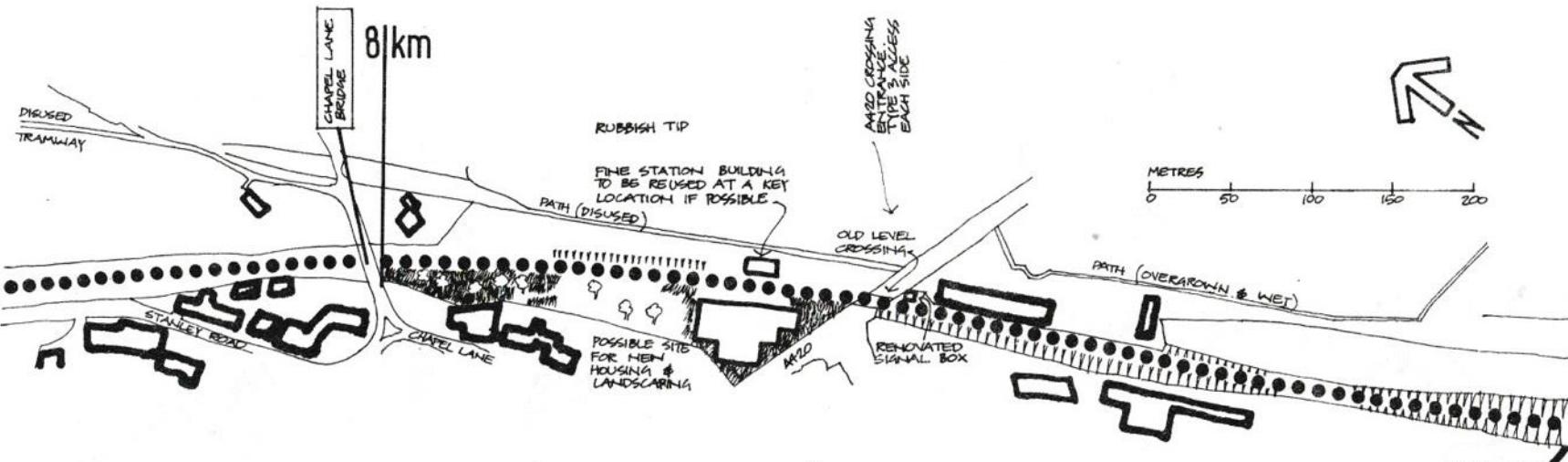


Tipping limestone dust near Bitton.



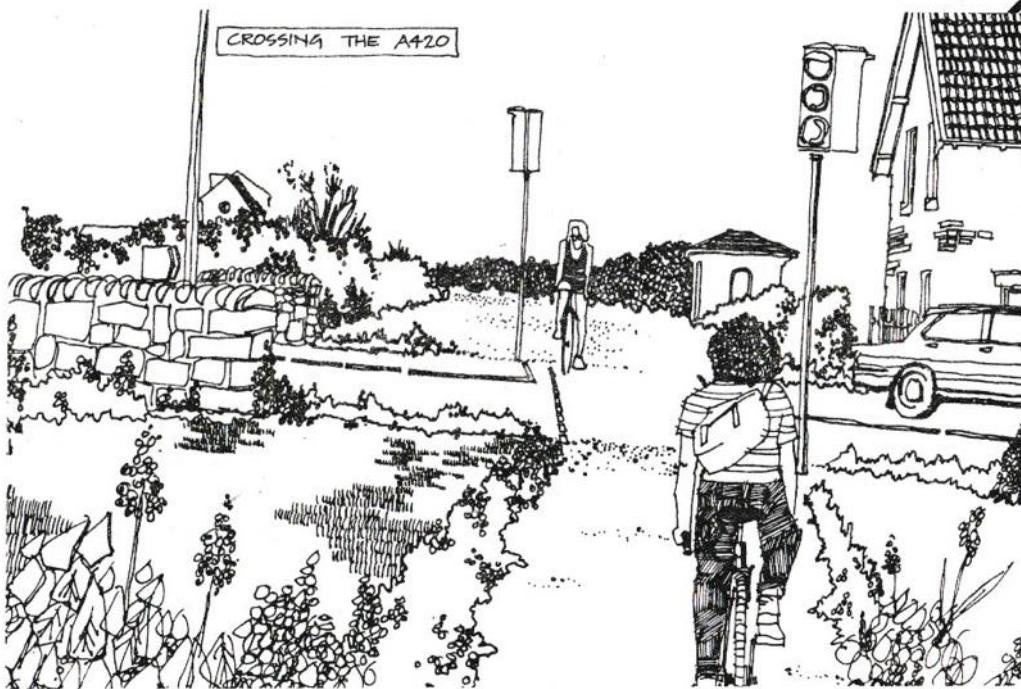
Raking out and shaping limestone dust near Bitton.



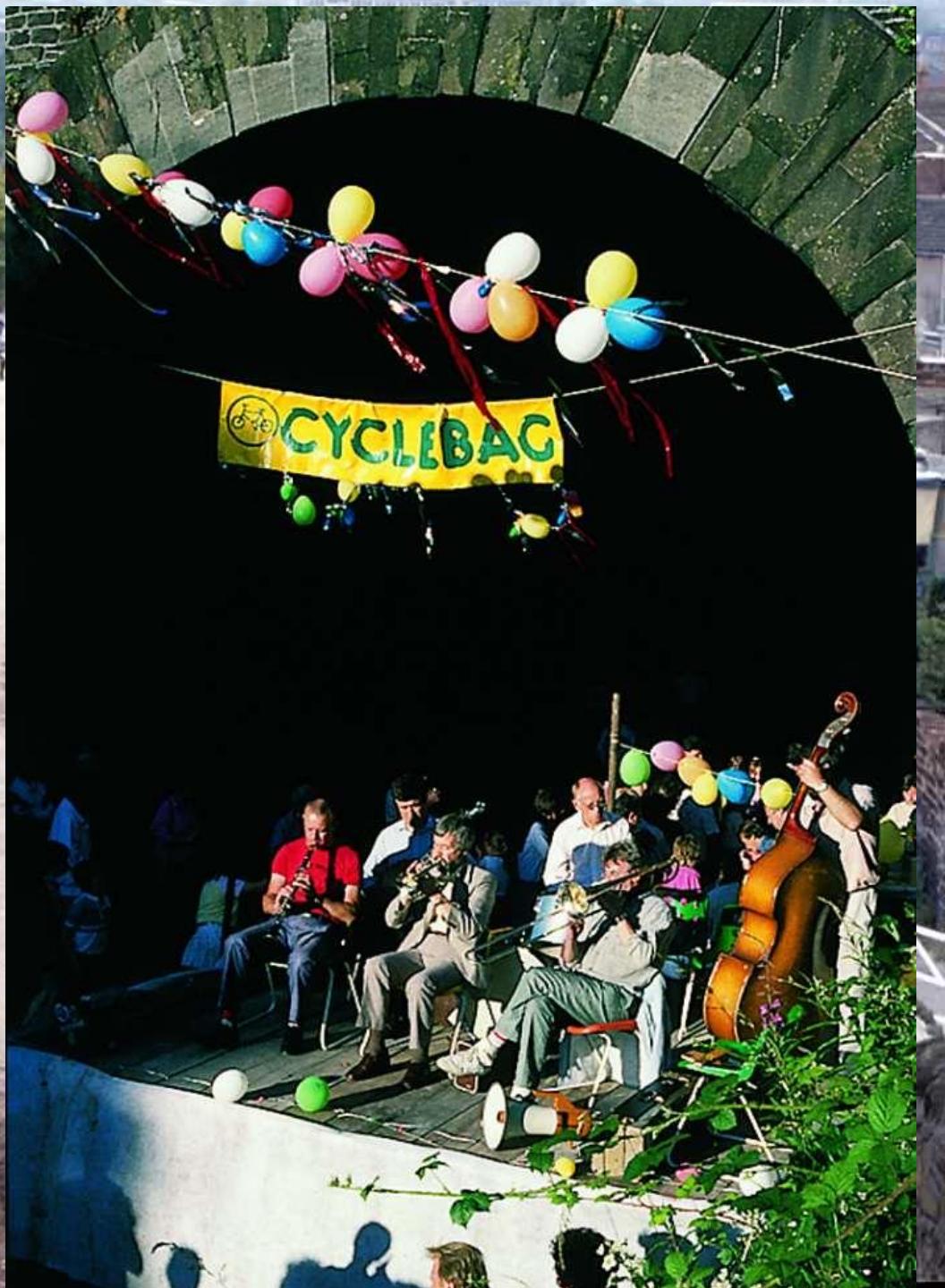


A 420 SEGREGATION & PRIORITY

Mention has already been made of the elements that together make up a successful primary route that encourages its use by many travellers. Of these, continuity and priority at road junctions are crucial. It is essential that the potential user perceives the route as direct, quick and safe. The latter is particularly important in the case of parents permitting their children to travel independently. Down through Warmley it would be possible to make use of the dismantled tramway rather than the railway for 2-3 kms. But except for the main crossing of the A420, the railway is grade segregated at every crossing with existing bridges, whilst the tramway is not. On these grounds the railway is to be preferred as the main route whilst the tramway may be kept as a wilder and much less disturbed footpath.

























sustrans

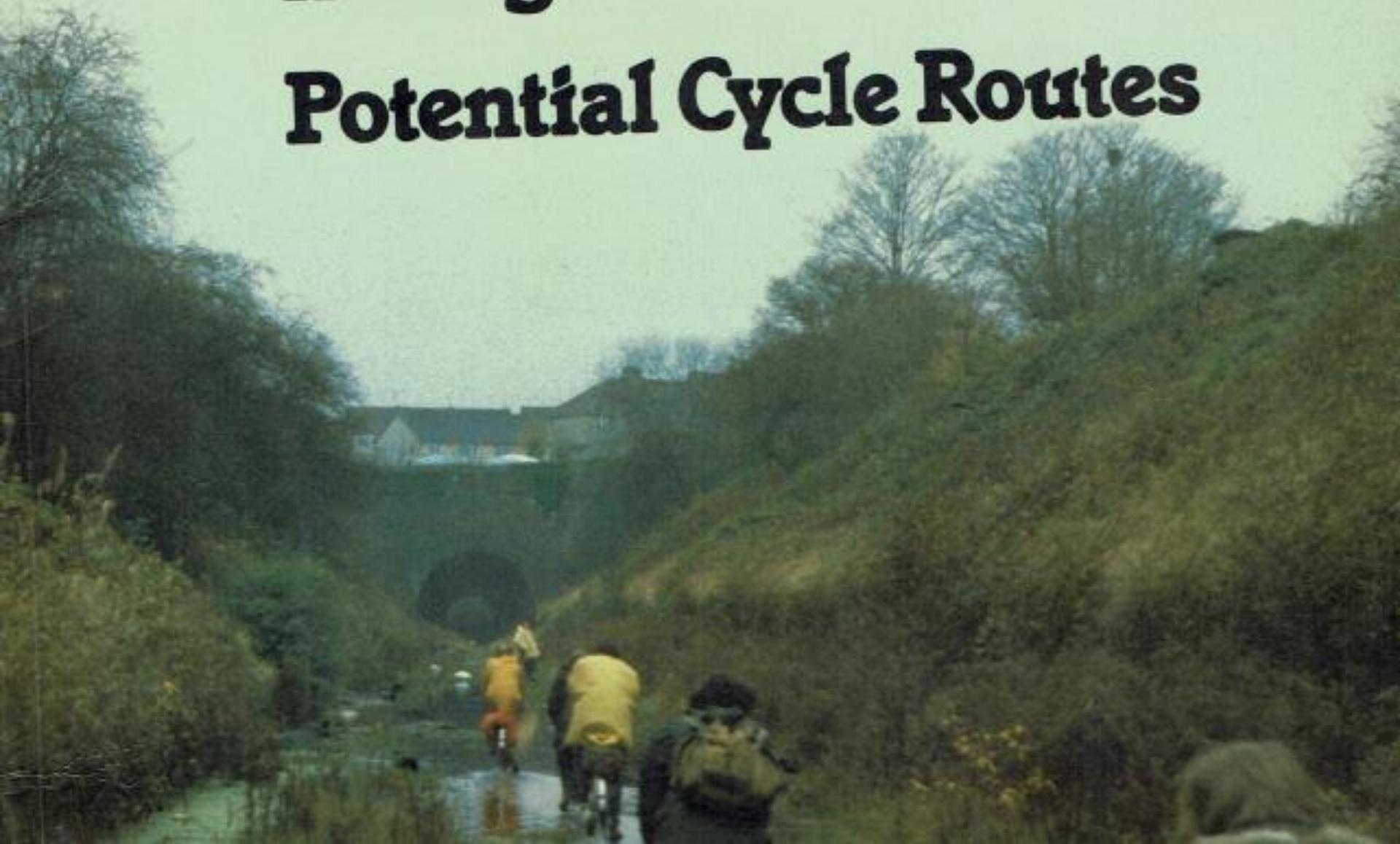




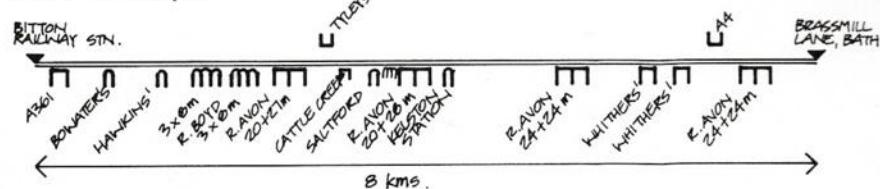


Study of Disused Railways in England and Wales

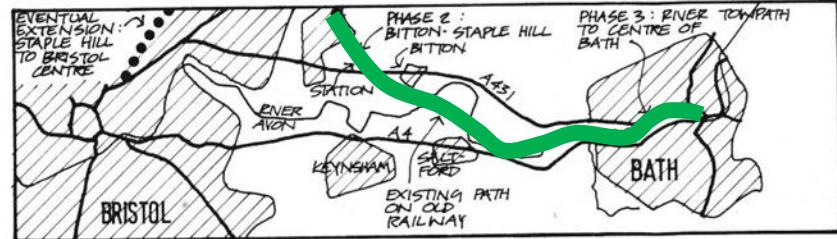
Potential Cycle Routes



Bitton to Bath Railway Path



LINE SCHEDULE OF BRIDGES



SCHEMATIC MAP



1. Tipping Limestone dust onto remaining ballast



2. Spreading dust and raking level



3. Compacting with twin drum vibrating roller



4. Cyclists under A4 road bridge

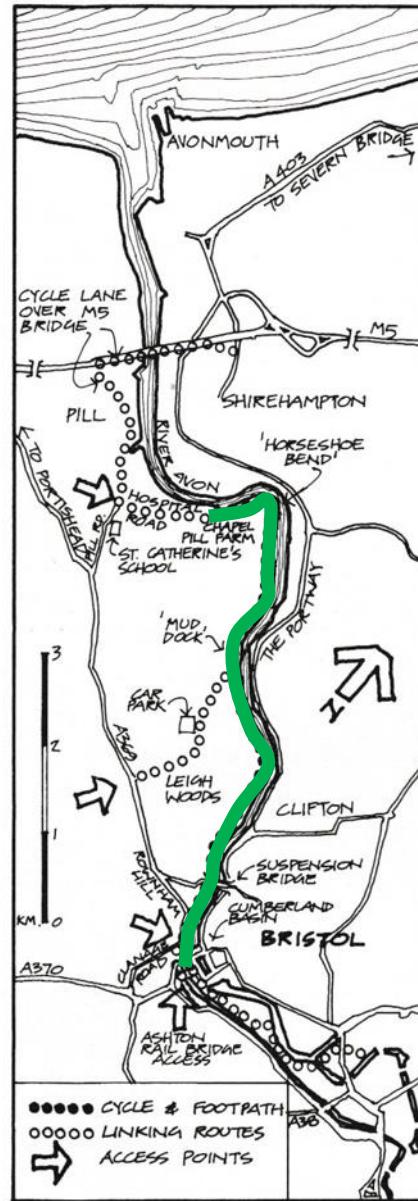


5. Pedestrians and cyclists near Avon bridge



6. Great British Bike Ride (1981) near Bitton

Ashton to Pill Path



1. Flood Bank beside River Avon before construction.



2. Stone laid on filter fabric on new path.



3. Finished path at high tide.







Bristol & Bath Railway Path



Kennet & Avon Canal



Devon C2C, Ham viaduct





Derby and Melbourne 1984



Lochwinnoch Loop Line – Glasgow to Ayr 1984









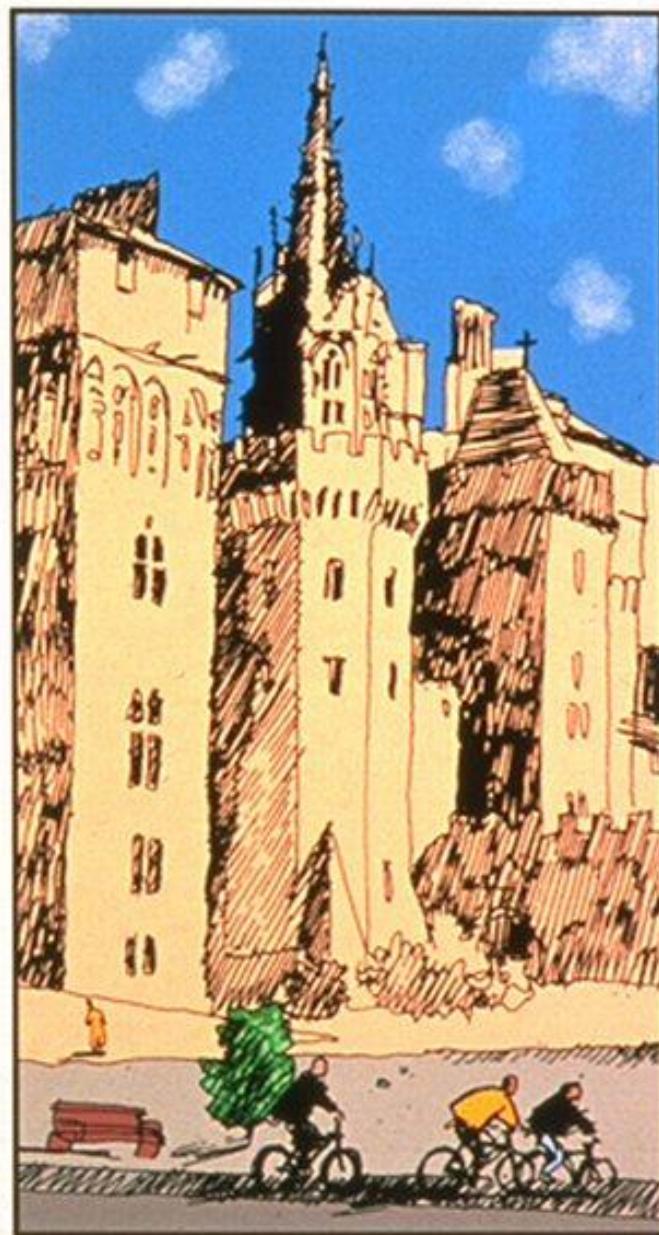


C2c Whitehaven to Sunderland 1992

A T L A N T I C S C Y C L E R O U T E



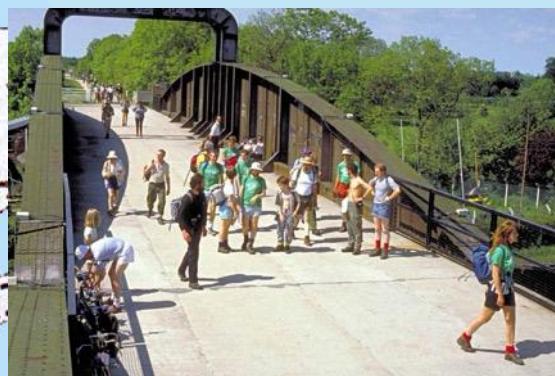
Proposed Welsh National Cycle Route



Proposed Welsh Route at Cardiff Castle



1992
Early work





Sept 1995
2,500 miles
by 2000
additional
4,000 miles
by 2005

The National Cycle Network





1995 Lottery fund award of £43.5m to create
2,500 miles of National Cycle Network



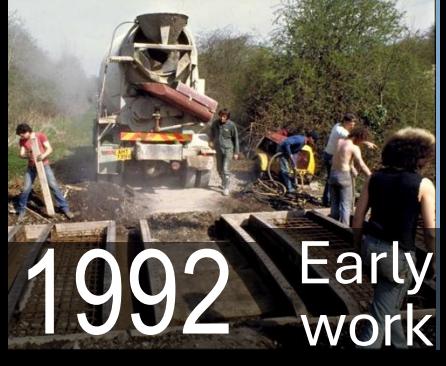












1992

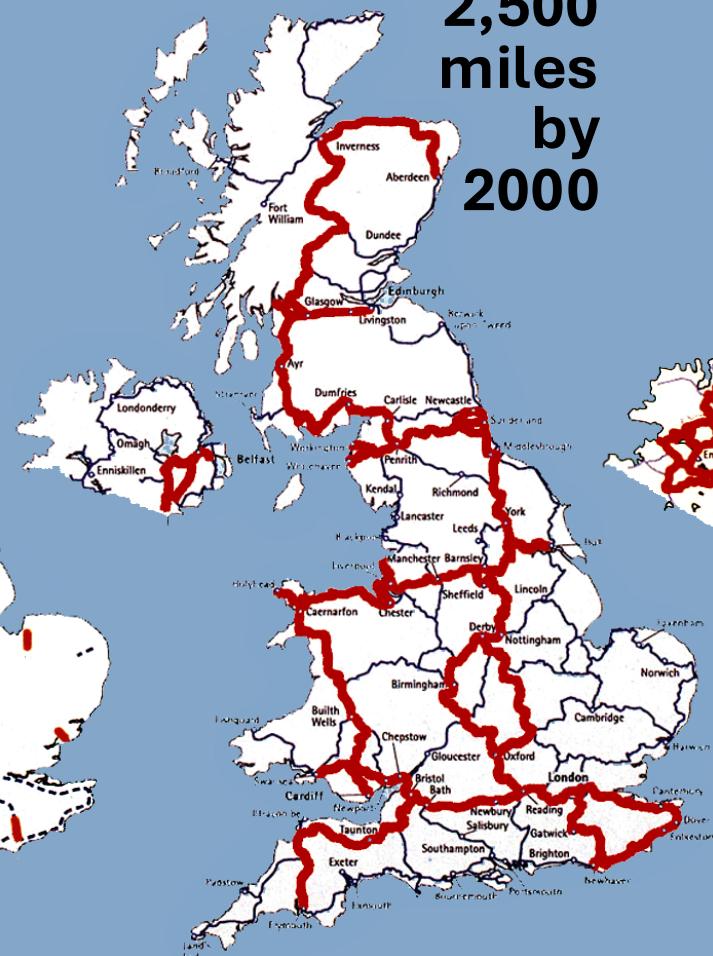
Early work



1995 Target:



2000 Achieved:



2,500 miles by 2000



5,000 miles

sustrans
JOIN THE MOVEMENT

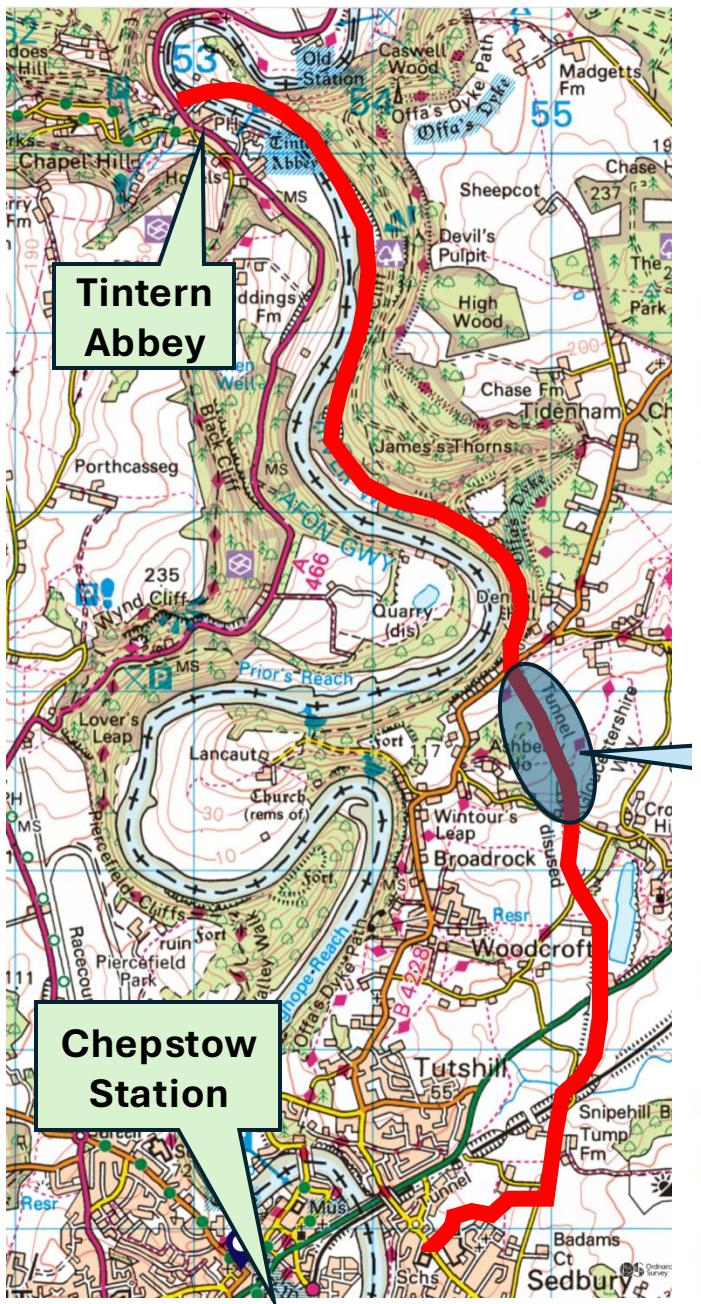












Selection of pictures from the works to make Tidenham Tunnel (1100m) into a part of the Wye Valley Greenway. Track lifting by John Davies, Surfacing by Hazel and Jefferies, and all other works including the design and installation of lighting by volunteers. Greenways and Cycleroutes during 2020





*Annual Camps set targets
which have to be met.
Permissions secured, funds in
place, challenges to complete*

*Camps draw local people in
to claim ownership and
form the core of ongoing
maintenance teams*







The Somerset Circle – a future asset to our region





Strawberry Line Greenway Project

Westbury and Easton Project

Notes, plans and detailed sketches prepared for construction of Phase 2 Erlon Lane to Ebbor Lane

Greenways and Cycleroutes February 2023



***View of Easton Cutting after January clearance by local volunteers
– main road bridge is visible in the distance.***













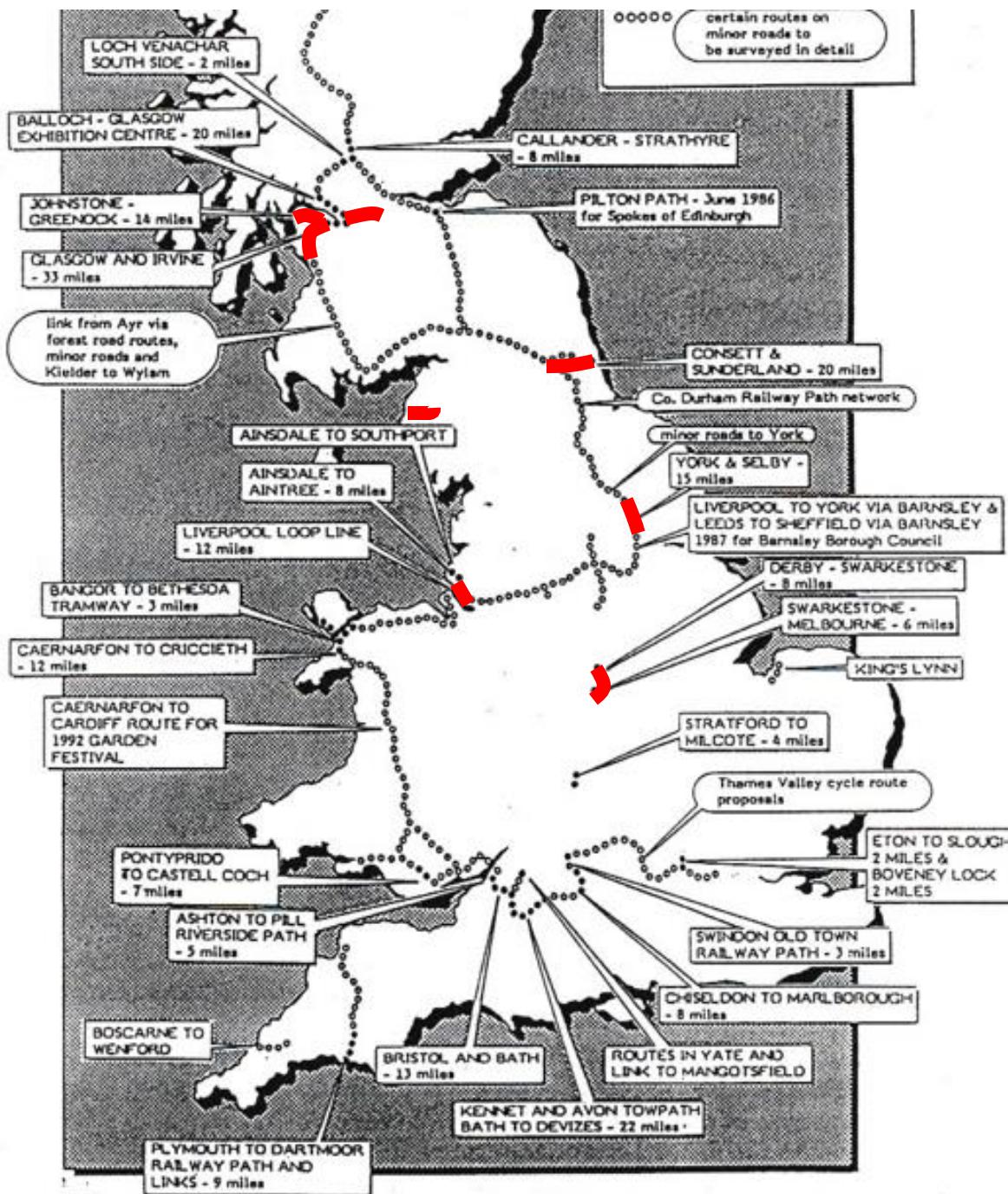


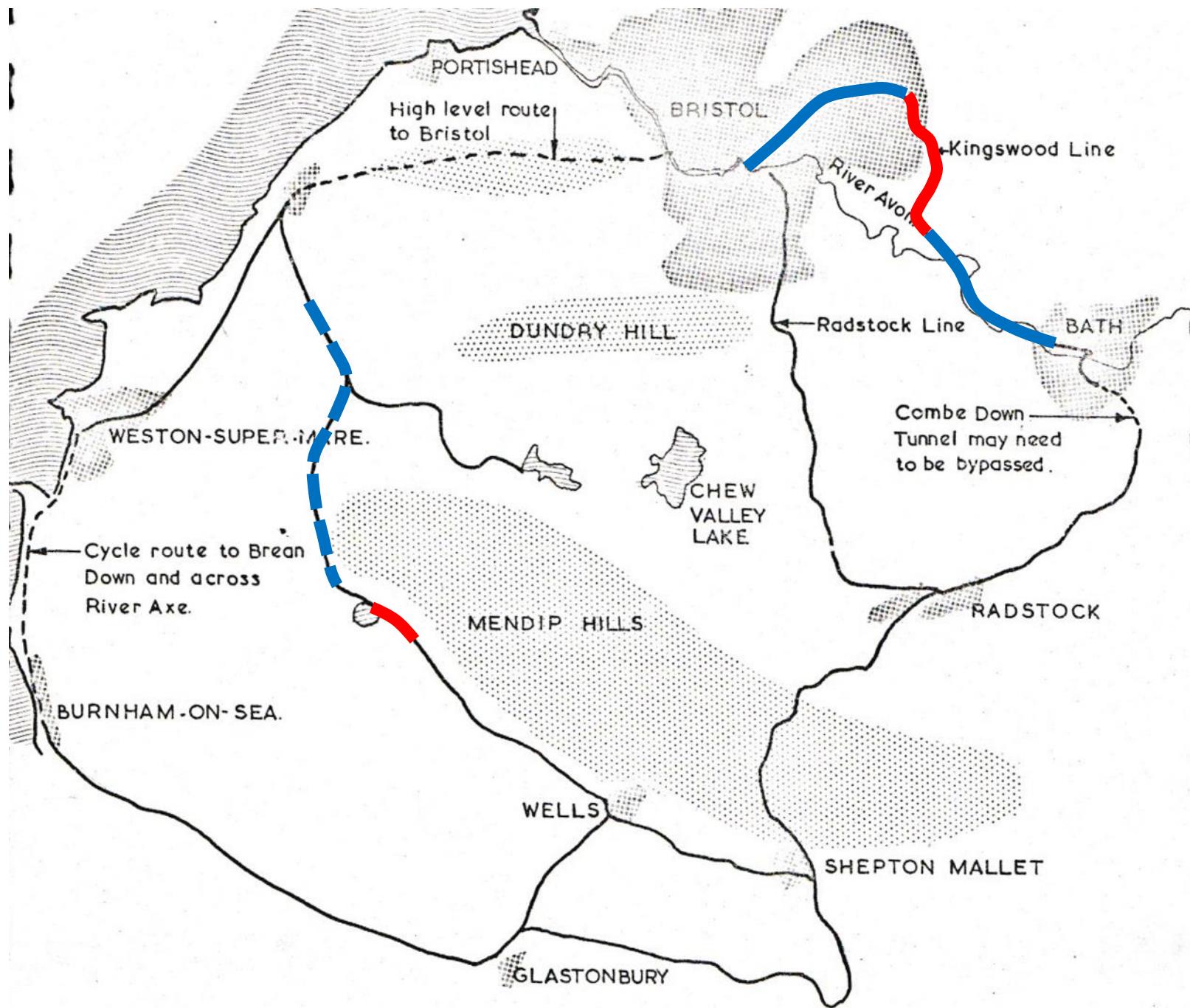




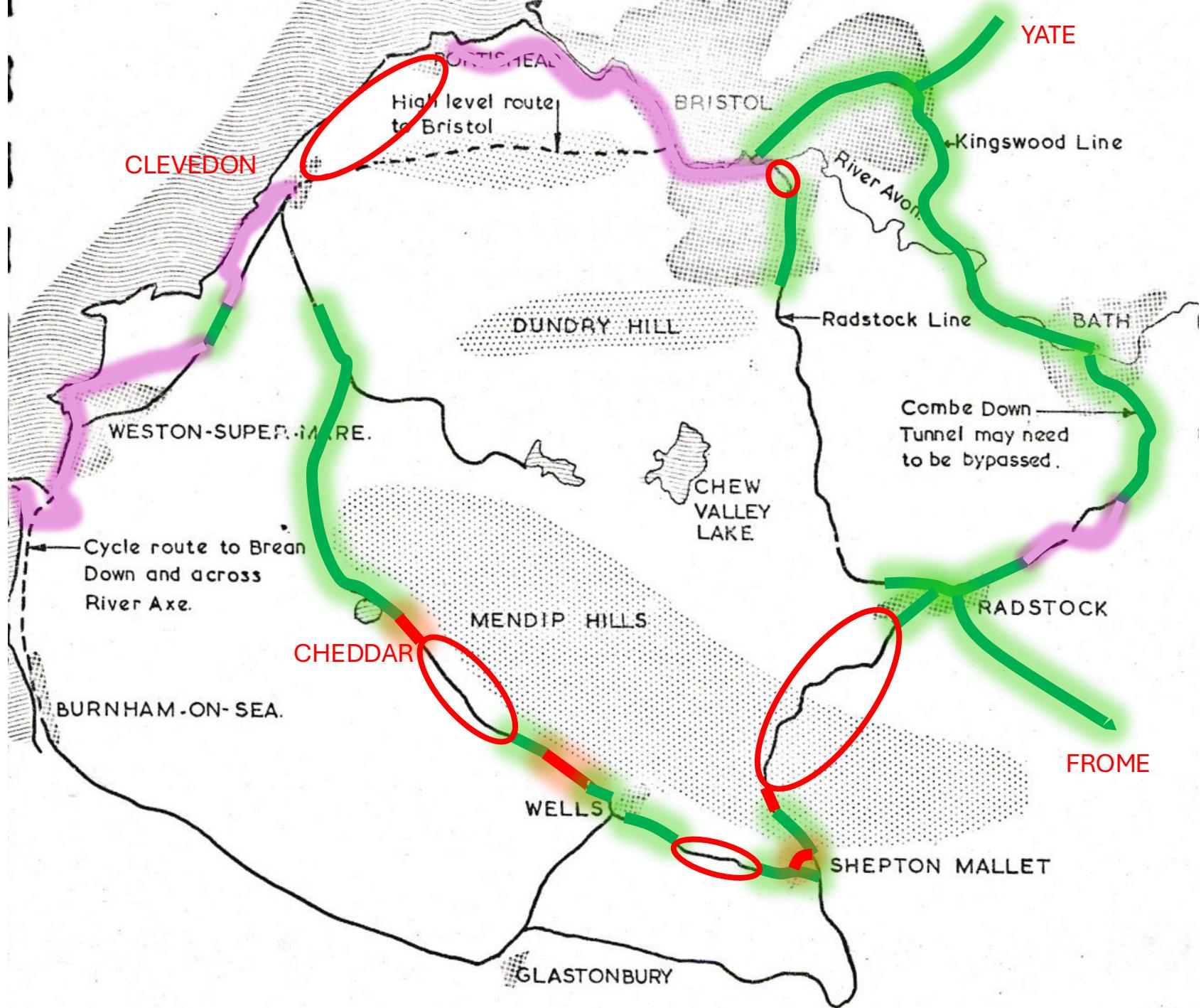






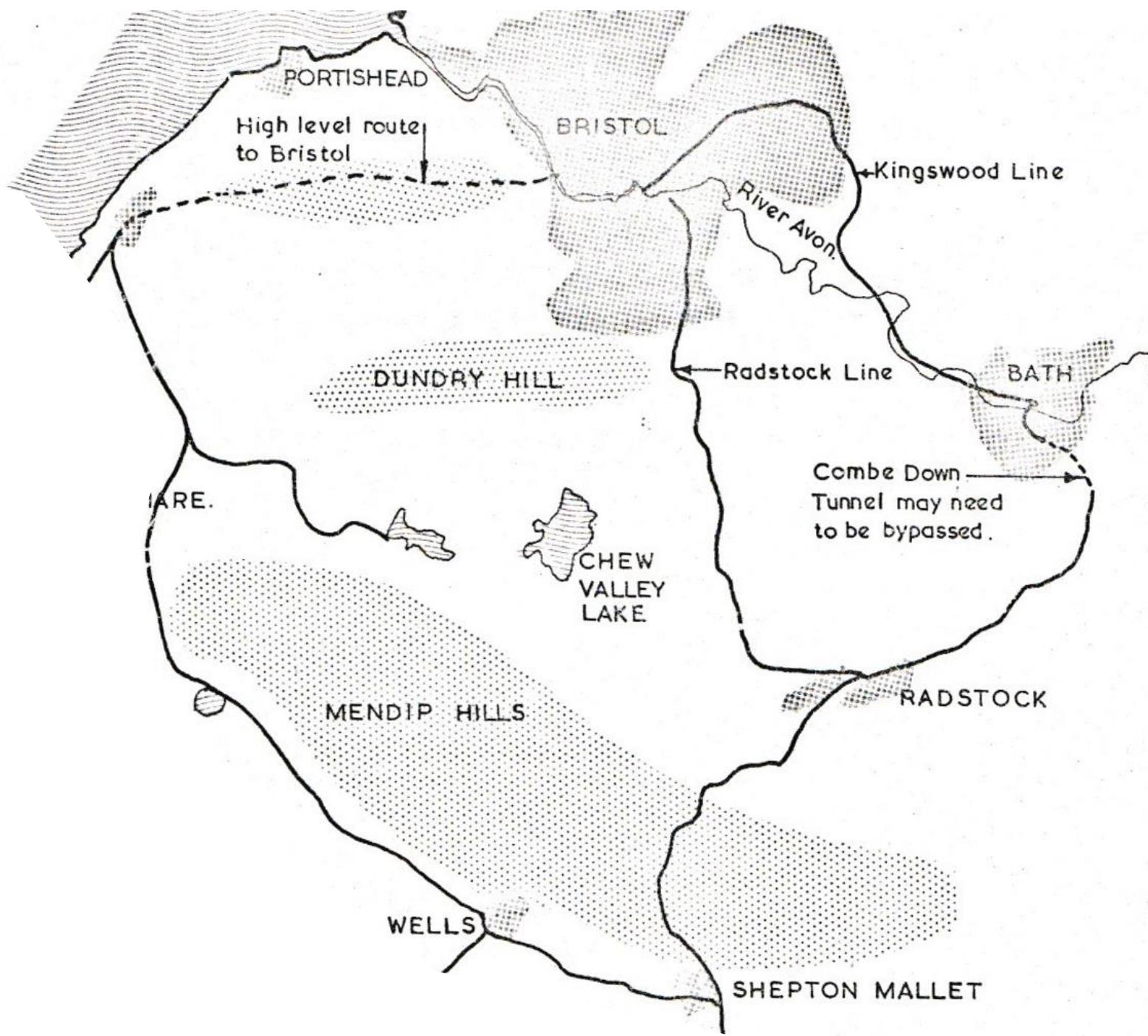


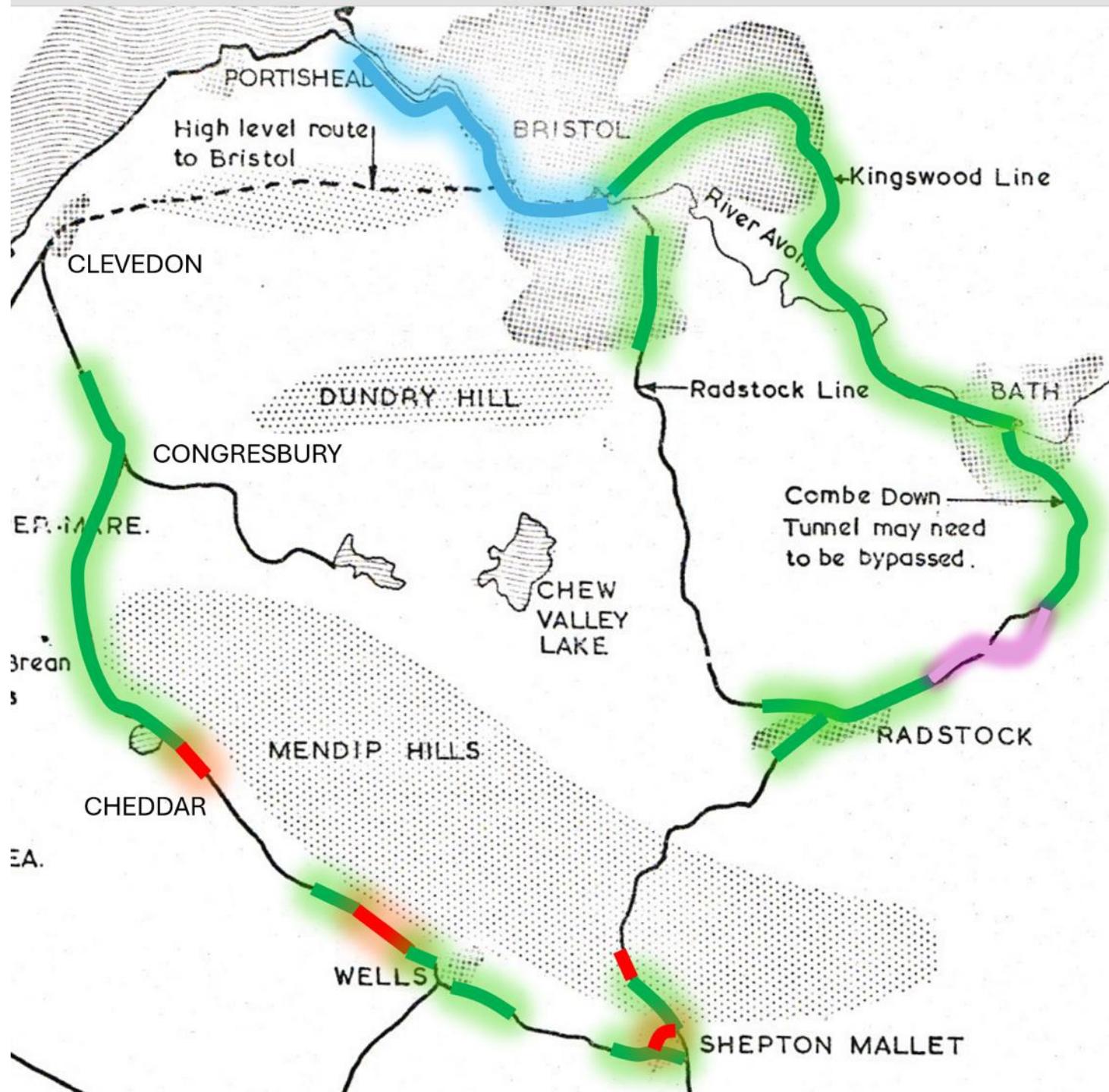


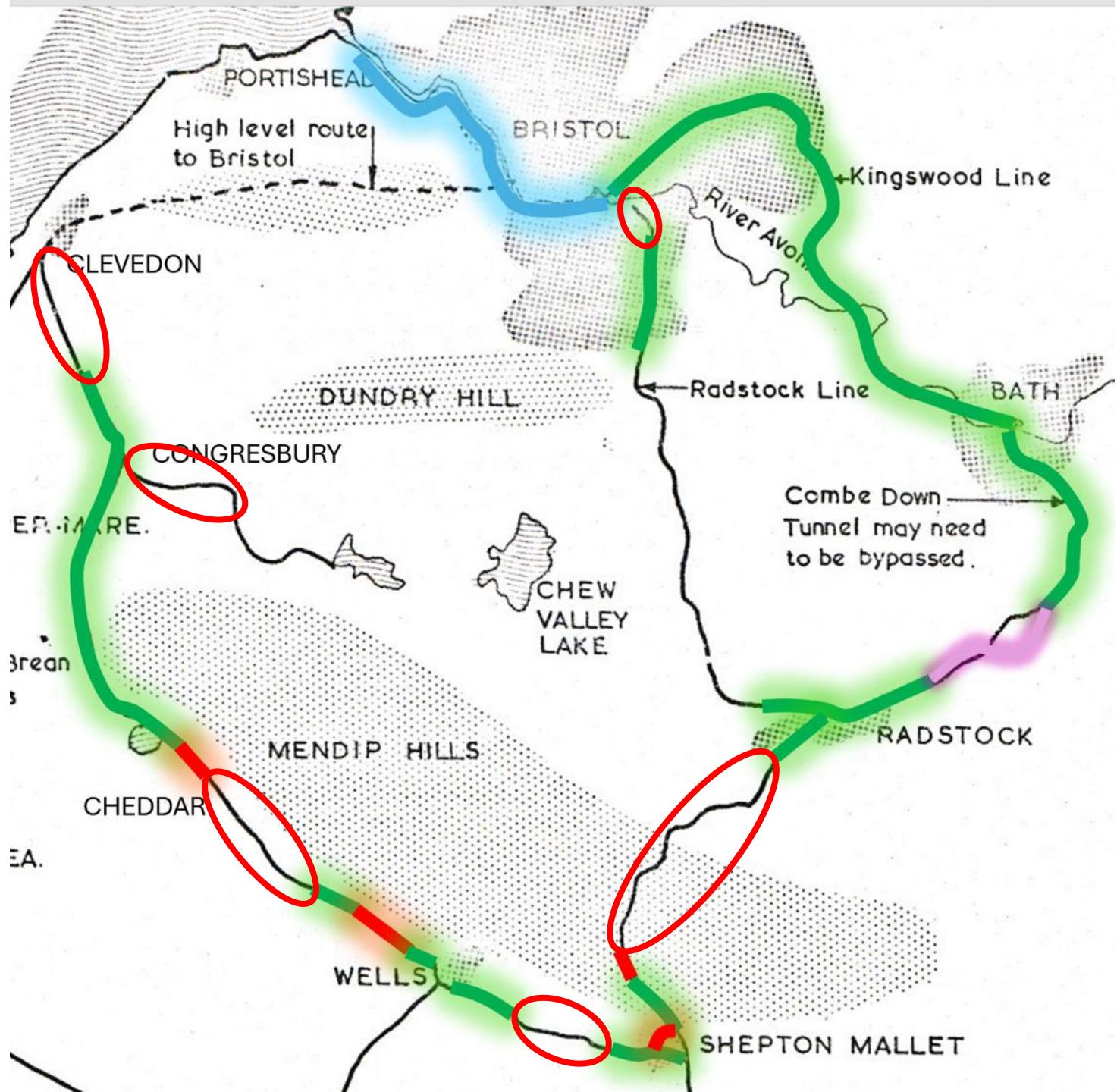


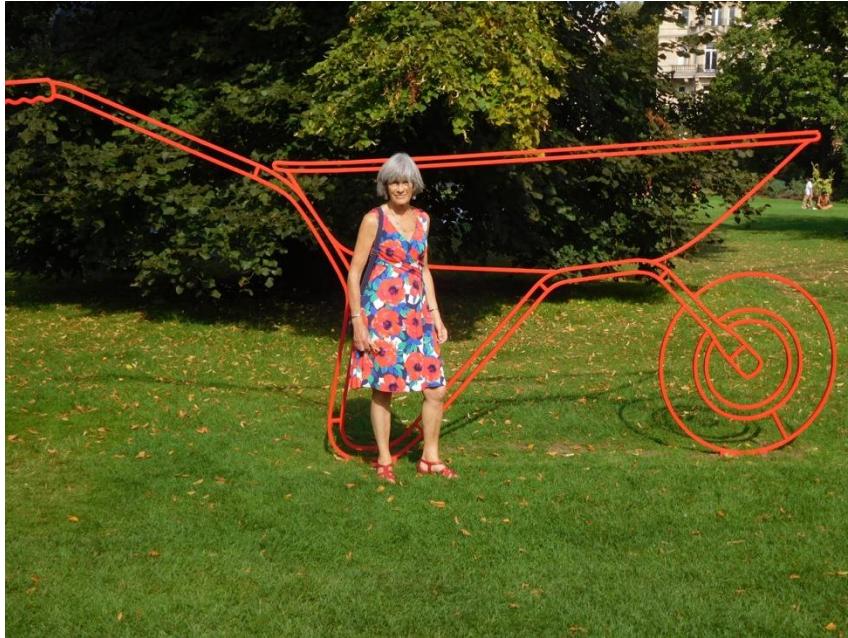
















Devon C2C, Ham viaduct