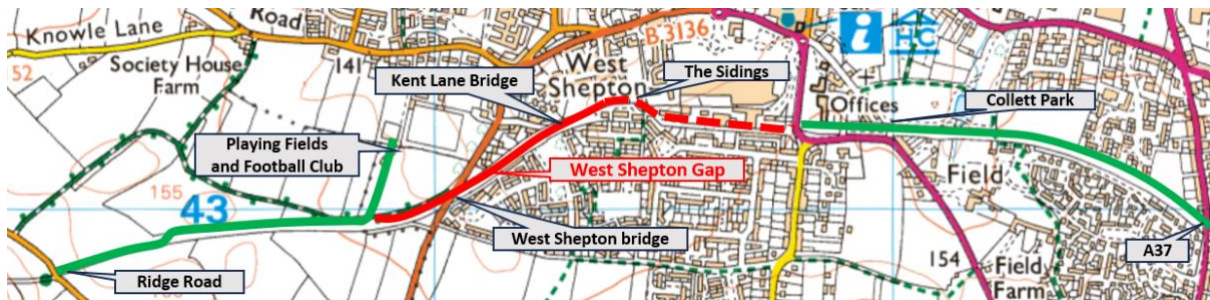


The Strawberry Line - The West Shepton Gap (playing fields to the Sidings and Cannard's Grave Road bridge). **Brief Project Note**

The West Shepton Gap runs for 1000m between Ridge Road and the playing fields path which was completed in March 23, and the Campus Link under the main road at Cannards Grave, which was opened also in March 23. Once the West Shepton gap is opened and in place, then Shepton Mallet will have a complete route from the A37 to Ridge Road nearly 3 km long This will be largely traffic free, except for the Sidings section which is a lightly trafficked residential road with a small amount of industrial access in the old station area.



Plan of Ridge Road to A37.

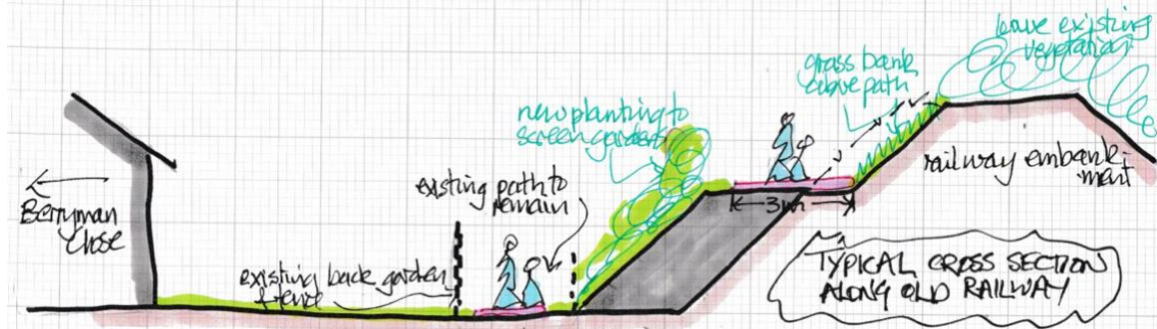
The completion of the West Shepton gap includes the installation of a new bridge over the main road and extensive earthworks to achieve a path with a gradient no steeper than 1: 20 suitable for wheelchair use. The works will be carried out by M and K Golding, local contractors who have built the recent paths in the area, all under the design and supervision of Greenways and Cyclerooutes Ltd, with the crucial support of local volunteer groups who have undertaken to coordinate with neighbours and other local interests.

Once completed, and when eventually linked up to Wells, this path will form the easternmost section of the Strawberry Line project, connecting all the way to Cheddar, Axbridge, Winscombe, Yatton Station and Clevedon



Strawberry Line Clevedon to Shepton Mallet

The works are designed to enhance the area and in effect extends the ground plan of the existing Queens Road Park. At the moment the disused railway is just a background to the path rather than an integral part of the park. The Path will connect with Kent Lane and then gradually ramp up the southside of the railway embankment to reach the planned new bridge over the West Shepton Road. The path will not run over the Kent Lane bridge, because if this happens, it will overlook adjacent residential areas and it will be difficult to make a connection for local people. Kent Lane bridge though, will be retained as a significant railway feature.



The project distinguishes between the sides of the cuttings and embankments which will be managed as an attractive background to the Queens Road houses and the areas which will left as wildlands. These will include much of the old trackbed, and the whole of the north slope overlooking nearby housing. Throughout the works the advice and recommendations of the project ecologist will be followed.



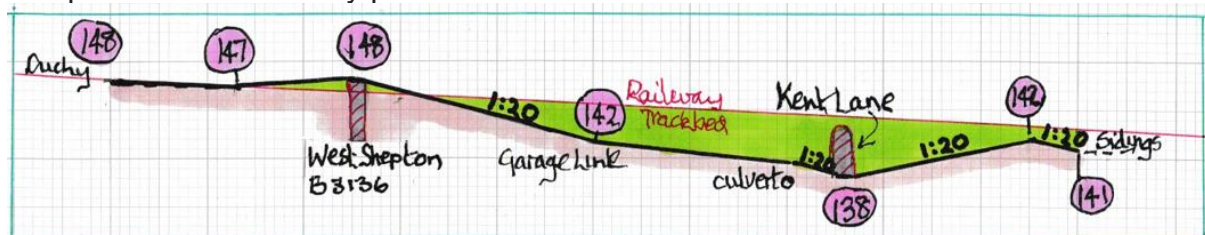
Plan showing the railway section from West Shepton (road) to the Sidings. The area in dark green will be left untouched to go wild. The area in light green where the ground will be worked during the construction will be planted as an extension of Queen's Road Park (shown in pale green). You will be able to get onto the path from the Sidings, at Kent Lane, and opposite the footpath from Berryman Close.

Work is expected to start early in February with the earthworks finished by April so that all the newly exposed banks can be seeded. The link from Kent Lane to The Sidings will be opened first, whilst the completion through to the Playing Fields will need to wait till the new bridge is in place.

If you have any queries or points of interest, please contact one of our local volunteer team or email the project engineer at john@johngrimshaw.co.uk
 Greenways and Cycloroutes with Somerset Council January 2024

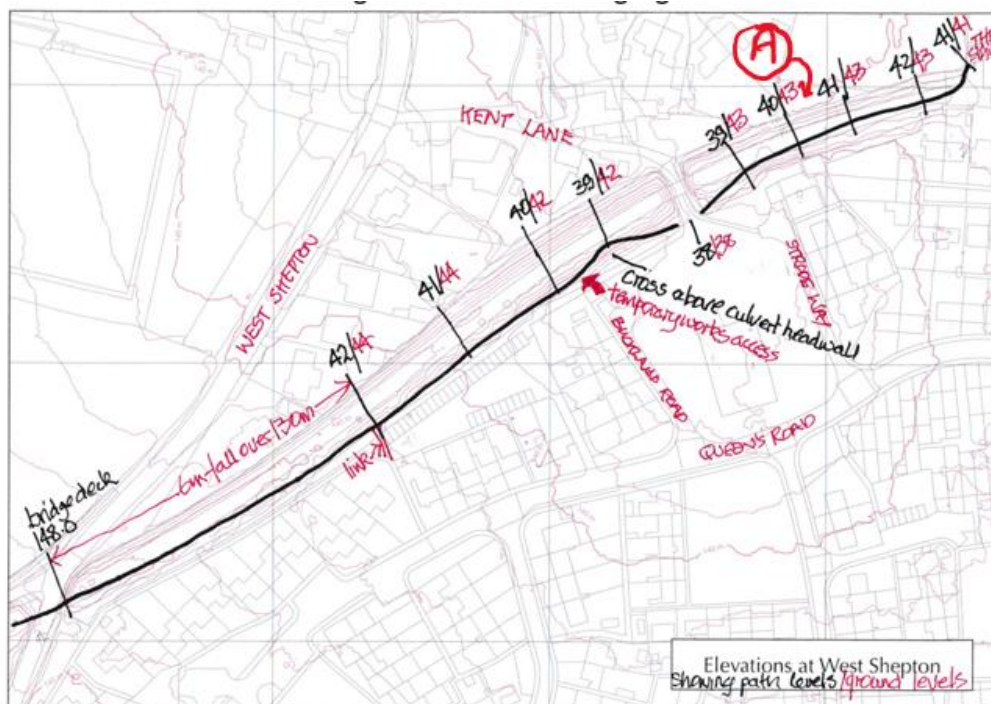
West Shepton gap - Duchy lands to the Sidings 460m to Kent Lane and 115m to the Sidings – *Working Details*

This section of the Strawberry Line is a particularly interesting piece of work. A new bridge deck is required to replacing the missing span at West Shepton and extensive earthworks are required to ramp up to this bridge, which will be raised some 2 m higher than the original road bridge to provide a 6 m clearance to the road below. From here, there is a 10 m drop to the level of Kent Lane, where we gain access to the local community, followed by a 4 m climb to reach the residential road at the Sidings. The Long section profile shows how the path levels were relate to the original track bed level, and underline the care needed to construct the path to be no steeper than 1: 20 at any point.

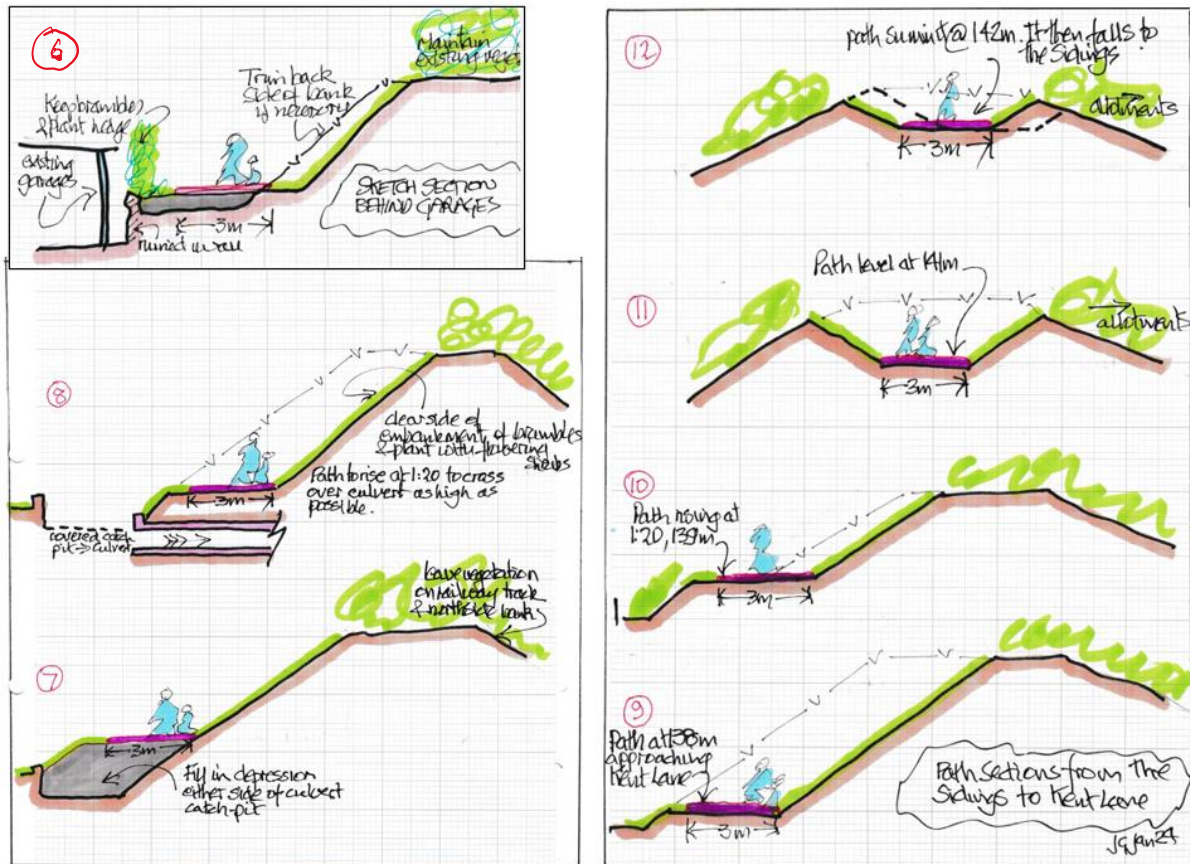


Long section (not to scale)

Section from Duchy lands to the Sidings. The plan and cross sections show the scheme as currently envisaged. Note that the exact arrangements at the West Shepton Bridge approaches will be decided once the approach earthworks are completed and we are certain of the extent of the gabions and retaining structures required. Also note that the works have been made somewhat more complex than originally envisaged on account of the park now being owned by Somerset Council rather than Mendip so the route cannot so conveniently bypass the culvert under the railway and embankment. In addition, the failure to acquire the triangle, 'A' next to the allotments makes for greater work managing the balance of excavation and fill.



Program for carrying out the works. The works access will be from the end of Buckland Road where the adjacent corner of the park with provide a convenient compound area.



Stage one- create a working base over sections 78&9. Here we have to excavate a 4 m wide bench across the top of the culvert, keeping a good 1 m cover over the crown of the culvert itself (section 8). The excavated material is to make up the ground in the two depressions either side of the inspection chamber, and any surplus is to be used to make a works road to the west. There is a lot of rubbish along the ground through to section one. This should be set to the side of the slope of embankment, and then when the whole access way is in place this rubbish to be taken west of section one. Note on timings for clearance of brambles and shrubs before March 1, this may require advance works



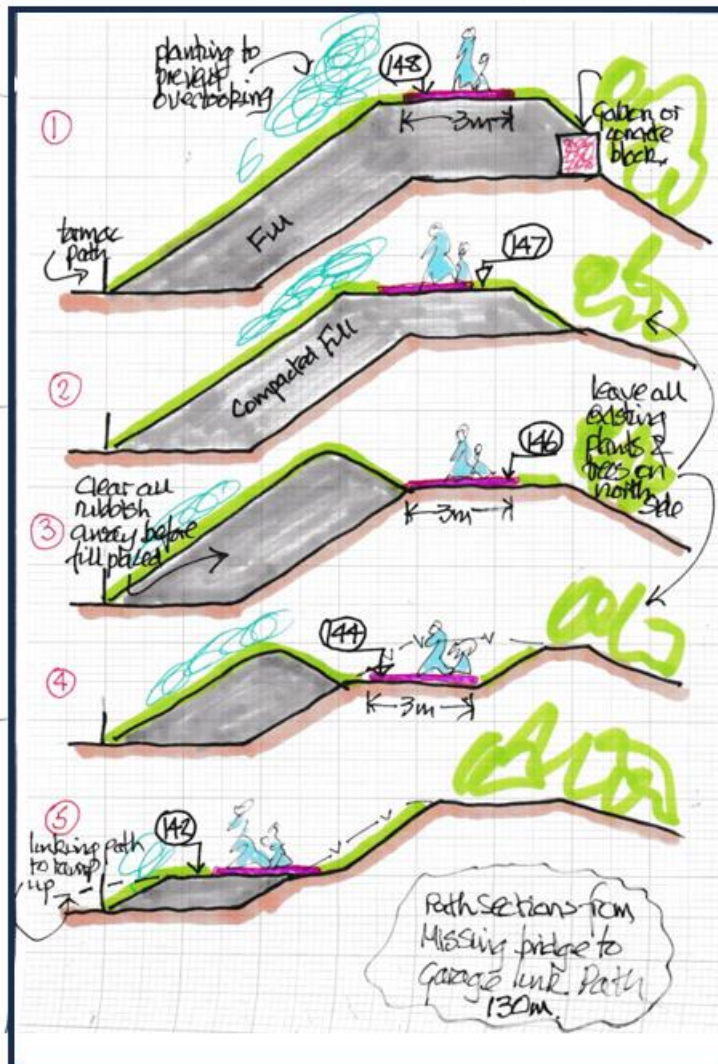
Stage two excavate Kent Lane to the Sidings (sections 9-12). Note that the section is almost wholly in excavation with little scope for fill, except in the small area at the end of Strode Way, where we own a section level ground which should be used for fill to make a privacy bank to minimise overlooking of the adjacent property

Plan of Strode Way boundary detail,

All this material needs to be carried across Kent Lane. Erect warning signs, either side of the crossing together with a chicane from rocks or blocks. The dumper driver to come to stop to ensure

that the path is clear before proceeding. The excavated material is to be used for the haul road (section 6). If we have sufficient material the path gradient can be eased from 1:20, and the overlooking of neighbours can be further protected by banks

Stage three construction of sections 1&2. The purpose of this work is to gain the



height to lead on to the replacement bridge and to gain width, so the extent of any retaining works is minimalised. Before work commences, erect, Herras fencing or similar to prevent any material inadvertently falling onto the West Shepton Road AND scaffold across the remaining bridge parapets with mesh. The fill should be well compacted as it is placed and if possible, the side slope should be finished suitable for grass seeding as soon as possible after completion. Once we know what the shape of the land is as it approaches the bridge we can draw up arrangements for wingwalls and revetments leading to the bridge abutments.

Note the high level overhead powerline which will require warning tapes to be in place during the works.

Note Bridge funding is still to be confirmed.

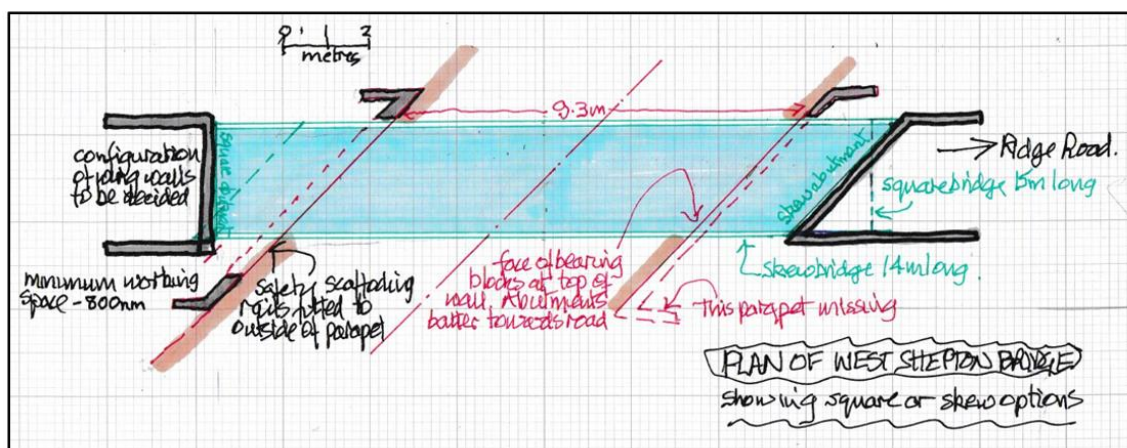
Stage four complete remainder of the path formation from section 2 to section 7, including the access link at section 5. Note any final surfacing to be completed after the bridge and its approaches are in place, except for sections 9 to 12, which should be completed with its final surface as soon as practical so that the public can enjoy this link

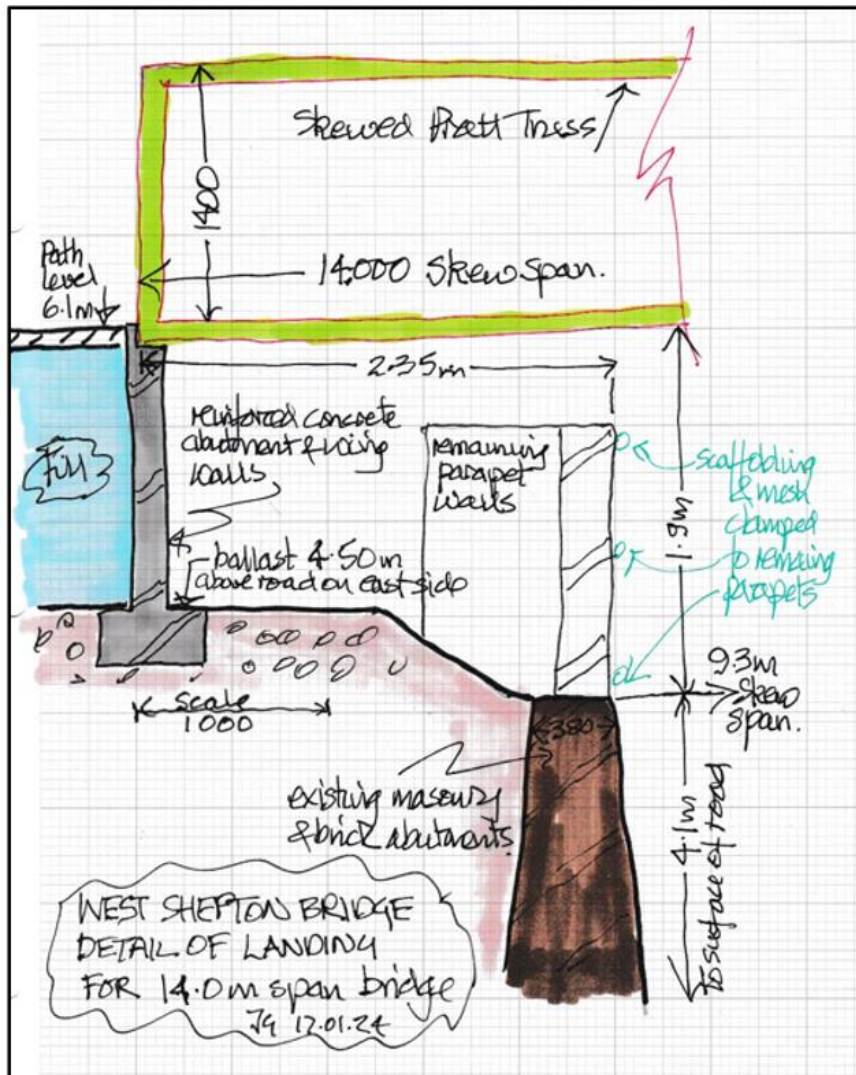
Stage Five Seed, all exposed slopes and carry out and certain other ancillary works eg to make the Kent Lane bridge safe, including parapet extensions, if necessary

Scale 100 metres

Strawberry Line Project
West Shepton Road crossing
Greenways and Cycleroutes January 2024

We are debating whether to go for a slightly longer bridge with square ends or to main with a skew version. This will become easier to visualise once the ground is cleared and the earthworks are in place on the Shepton side. It might even be worthwhile slightly slewing the bridge in a clockwise direction. The sketch plan below shows options.





Sketch showing proposed arrangement of bridge and new concrete abutment in relation to the remaining railway abutments.

Greenways and Cycle routes January 19th 2024